

# Montracon Box Vans



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# Features



Our vans can be fitted with flush, barn or roller shutter doors at the rear, with a further option of having flush doors incorporated along either or both sides. Barn doors feature robust, large diameter lock rods, flush doors are offered for better presentation of livery, and shutters, including wide lathe shutters, are offered for quicker dock loading/unloading to speed multi-drop operations.



'High gloss' finish inside and out helps to maintain a clean, neat appearance, with an ample kick strip providing scuff protection during loading/unloading. Load restraint rails are side panel mounted.



Montracon offers a choice of lighting packages, including LED that are increasingly becoming the preferred choice for improved performance and longevity.



Montracon fits the full range of tail lifts - pillar, tuck or slide away - offered by the major manufacturers for any application you require.



To ease loading/unloading, the trailer deck is easily raised or lowered at the rear to match dock height. Automatic 'return to ride' restores the deck to normal running height on departure.

Carefully positioned reinforced buffers minimise the risk of damage when reversing up to a loading bay, for example.

Vertically laminated hardwood floor fitted as standard with either steel or aluminium chequer dump plates fitted flush or overlaid.



With an aluminium skin as standard, Montracon offer a GRP translucent roof or for loads that are susceptible to heat or moisture then a 30mm insulated roof is also available.

Developed to aid safety, MAVIS (Montracon Articulated Vehicle Interconnecting System) allows coupling/uncoupling from either side of the trailer - eliminating any need to clamber onto the tractor cat walk.

A variety of stow away ladders eases entry/exit to and from the body.

# Box Vans

*A comprehensive classic.*



Of all the European manufacturers, Montracon offers one of the most comprehensive range to ensure that vans meet the broadest and most diverse range of applications. Having been successfully tried and tested, Montracon's vans are adaptable, strong, flexible, and reliable.

## Why Montracons'?

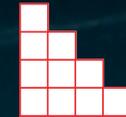


Renowned for Quality, Strength, & Longevity



Enviably proven performance

**Extensive warranty ✓**



Single & Double Deck



Straight & Step Frames



Overall heights of up to 4.8m

Montracon Box Vans are GRP panelled and are supplied as either a straight or step frame, the latter primarily for high cube operations. Trailers are mounted on either a single axle, tandem, or triaxle bogies for operation at up to 44 tonnes GTW, and beyond.

In addition, Montracon offers a full range of aerodynamic options from the fully specified FST (fuel saving trailer) to the inclusion of specific, individual features.

Thereafter, options are fine tuned to meet more exacting requirements and to maximise your operating flexibility and your fleet efficiency.

# Fuel Saving

Montracon has developed a range of fuel saving options that can trim consumption by up to 10% - depending on operating conditions and the level of options chosen. Top of the range is the purpose designed FST (fuel saving trailer). It uses aerodynamic design and vortex generator technology to provide attached air flow along the roof and sides, directing it into the base area immediately behind the trailer to reduce drag.



Depending on operating conditions, the Montracon FST (fuel saving trailer) can trim the fuel bill of combinations using a standard 4.2m high trailer by up to 10%.



Side skirts promote attached air flow along the trailer sides. Vortex generators direct attached air flow to the base drag area immediately behind the trailer to reduce drag.



A purpose designed front headboard promotes efficient air flow around the trailer from the tractor units cab mounted deflector.



A sloped roof design promotes an attached air flow to improve the aerodynamic envelope & the fuel economy achieved with the max height trailer. Savings as high as 10% have been reported for overnight motorway trunking operations.

# More features →



Rear door/tail lift arrangement designed to meet specific retail delivery requirements.



Designed to meet the specific needs of an outside broadcast provider.



Engineered to accommodate racing cars and crew, yet be low enough to access the pits via tunnels under the race track.



Internally installed lift, with fore/aft movement to ease loading and unloading capability.